



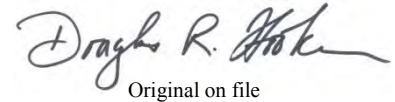
REGIONAL REVIEW NOTICE

Atlanta Regional Commission • 40 Courtland Street NE, Atlanta, Georgia 30303 • ph: 404.463.3100 • fax: 404.463.3105 • www.atlantaregional.com

DATE: June 5, 2015

ARC REVIEW CODE: R15060201

TO: CEO Lee May
ATTN TO: Shawanna Qawi, Senior Planner
FROM: Douglas R. Hooker, Executive Director
RE: Development of Regional Impact Review – Notification Only



Original on file

The Atlanta Regional Commission (ARC) has completed a preliminary regional review of the following development. ARC reviewed this development with regard to conflicts to regional plans, goals, and policies and impacts it might have on the activities, plans, goals, and policies of other local jurisdictions as well as state, federal, and other agencies. The preliminary report does not address whether the development is or is not in the best interest of the local government.

Name of Proposal: Decatur Crossing Phase 1 and 2

Review Type: DRI – Notification Only **Submitting Local Government:** DeKalb County

Date Opened: June 5, 2015 **Deadline for Comments:** June 15, 2015 **Date to Close:** June 15, 2015

Description: The proposed developments are located in DeKalb County, at the intersection of Scott Boulevard and North Decatur Road. Decatur Crossing Phase 1, on 4.67 acres at the corner of Scott Boulevard and North Decatur Road, will include 250 multi-family residential units and 2000 square feet of retail space and 15,000 square feet of leasing and amenity space. Phase 1 is located on the site of the Decatur Crossing Phase 2, on 15.45 acres immediately to the east of Decatur Crossing Phase 1 will include 450 multi-family residential units, 80,000 square feet of retail space and 15,000 square feet of office space. In total, the Decatur Crossing projects, located on 20.12 acres, will include 600 multi-family residential units, 90,000 square feet of retail space, 15,000 square feet of office space and 15,000 square feet of leasing/amenity space.

PRELIMINARY COMMENTS: According to the ARC Unified Growth Policy Map (UGPM) and the Regional Development Guide (RDG), the proposed development is located within the Maturing Neighborhoods area of the region. Additionally, the proposed development is located with the Medline Livable Centers Initiative (LCI) Study area. As such, the development proposal should be consistent with the plans, policies and recommendations within the LCI plan.

The RDG states that Maturing Neighborhoods are areas in the region characterized by older neighborhoods that include both single and multifamily development, as well as commercial and office uses at connected key locations. Though commercial and office buildings are aging, they nonetheless are often incorporated into neighborhoods, providing an active mix of uses and amenities. Maturing neighborhoods are denser than established suburbs and the development pattern is more similar to that of pre-1970s urban development. These areas represent the part of the region that is facing infill and redevelopment pressures. In many cases, the infrastructure is in place to handle the additional growth, however in some areas, infrastructure is built out with limited ability to expand. This may constrain the amount of additional growth possible in certain areas. Many arterial streets in this area are congested due to their use as a regional route for commuters. Limited premium transit service is available in these areas.

The demand for infill development, redevelopment and adaptive reuse of existing buildings in this area should be balanced with the preservation of existing single family neighborhoods. Consideration should be given to the need for additional usable parks and greenspace close to residents, as well as developing and maintaining pedestrian-friendly amenities such as trails and sidewalks. Maturing Neighborhoods are areas

where people are aging in place. These areas should integrate Lifelong Communities principles to ensure options for housing and transportation in the community that support older adults that want to remain in their communities for a lifetime.

The six-legged intersection of Scott Boulevard, North Decatur Road and Medlock Road presents significant challenges to area motorists, bicyclists, and pedestrians. DeKalb County, GDOT and the developers should work together to ensure safe pedestrian crossings are provided to and from the development site, across the roads bounding the development site, as well as through the six-legged intersection. This may include, but is not limited to, sidewalks, bike paths, pedestrian crossing signals and crosswalks. The specific facility type and technology depends on local conditions.

Additionally, DeKalb County and the developer should identify potential points of inter-parcel access and preserve these points for future connections. While access might not be constructed during the development of the proposed developments, the design of the development should not preclude this access from occurring in the future.

THE FOLLOWING LOCAL GOVERNMENTS AND AGENCIES RECEIVED NOTICE OF THIS REVIEW:

ARC COMMUNITY DEVELOPMENT
ARC RESEARCH & ANALYTICS
GEORGIA DEPARTMENT OF NATURAL RESOURCES
METRO ATLANTA RAPID TRANSIT AUTHORITY

ARC TRANSPORTATION ACCESS & MOBILITY
ARC AGING & HEALTH RESOURCES
GEORGIA DEPARTMENT OF TRANSPORTATION
DEKALB COUNTY

ARC NATURAL RESOURCES
GEORGIA DEPARTMENT OF COMMUNITY AFFAIRS
GEORGIA REGIONAL TRANSPORTATION AUTHORITY
CITY OF DECATUR

If you have any questions regarding this review, Please contact Jon Tuley at (404) 463-3307 or jtuley@atlantaregional.com. This finding will be published to the ARC website.

The ARC review website is located at: <http://www.atlantaregional.com/landuse>.



REGIONAL REVIEW NOTIFICATION

Atlanta Regional Commission • 40 Courtland Street NE, Atlanta, Georgia 30303 • ph: 404.463.3100 • fax:404.463.3105 • www.atlantaregional.com



DEVELOPMENT OF REGIONAL IMPACT – NOTIFICATION ONLY REQUEST FOR COMMENTS

Instructions: The project described below has been submitted to this Regional Commission for review. We would like to consider your comments on this proposed development in our review process. Therefore, please review the information about the project included on this form and give us your comments in the space provided. The completed form should be returned to the RC on or before the specified return deadline.

Preliminary Findings of the RC: **Decatur Crossing Phase 1 and 2** *See the Preliminary Report.*

Comments from affected party (attach additional sheets as needed):

Individual Completing Form:

Local Government:

Department:

Telephone: ()

Signature:

Date:

Please return this form to:

Jon Tuley, Atlanta Regional Commission
40 Courtland Street NE
Atlanta, GA 30303
Ph. (404) 463-3307 Fax (404) 463-3254
jtuley@atlantaregional.com

Return Date: *June 15, 2015*

ARC STAFF NOTICE OF REGIONAL REVIEW AND COMMENT FORM

DATE: June 5, 2015

ARC REVIEW CODE: R15060201

TO: ARC Community Development, Natural Resources, Transportation Access and Mobility, Research and Analytics, and Aging and Health Resources Division Chiefs

FROM: Jon Tuley, Extension: 3-3307

Reviewing staff by Jurisdiction:

Community Development: Tuley, Jon **Transportation Access and Mobility:** Studdard, Daniel

Natural Resources: Santo, Jim **Research and Analytics:** Skinner, Jim

Aging and Health Resources: Rader, Carolyn

Name of Proposal: Decatur Crossing Phase 1 and 2

Review Type: Development of Regional Impact - Notification Only

Description: The proposed developments are located in DeKalb County, at the intersection of Scott Boulevard and North Decatur Road. Decatur Crossing Phase 1, on 4.67 acres at the corner of Scott Boulevard and North Decatur Road, will include 250 multi-family residential units and 2000 square feet of retail space and 15,000 square feet of leasing and amenity space. Phase 1 is located on the site of the Decatur Crossing Phase 2, on 15.45 acres immediately to the east of Decatur Crossing Phase 1 will include 450 multi-family residential units, 80,000 square feet of retail space and 15,000 square feet of office space. In total, the Decatur Crossing projects, located on 20.12 acres, will include 600 multi-family residential units, 90,000 square feet of retail space, 15,000 square feet of office space and 15,000 square feet of leasing/amenity space.

Submitting Local Government: DeKalb County

Date Opened: June 5, 2015

Deadline for Comments: June 15, 2015

Date to Close: June 15, 2015

Response:

- 1) ☐ Proposal is CONSISTENT with the following regional development guide listed in the comment section.
- 2) ☐ While neither specifically consistent nor inconsistent, the proposal relates to the following regional development guide listed in the comment section.
- 3) ☐ While neither specifically consistent nor inconsistent, the proposal relates to the following regional development guide listed in the comment section.
- 4) ☐ The proposal is INCONSISTENT with the following regional development guide listed in the comment section.
- 5) ☐ The proposal does NOT relate to any development guide for which this division is responsible.
- 6) ☐ Staff wishes to confer with the applicant for the reasons listed in the comment section.

COMMENTS:



MEMORANDUM

TO: Jon Tuley, Land Use Division

FROM: Daniel Studdard, Transportation Access and Mobility Division

DATE: June 3, 2015

SUBJECT: **Transportation Division Review**
Project: Decatur Crossing Phase 1 and 2
County: DeKalb
Location: At the intersection of Scott Boulevard and North Decatur Road

cc: David Haynes
TAMD

The Transportation Access & Mobility Division has reviewed the trip generation statement that was prepared by the Foresite Group on behalf of Fuqua Development, LLC, the developer of Decatur Crossing. This proposed development is being considered for review under the DRI notification only threshold requirements.

Decatur Crossing Phase 1 and Phase 2 is located in DeKalb County, on the east side of the intersection of Scott Boulevard/US 29/US 78/SR 8 and North Decatur Road. Decatur Crossing Phase 1, on 4.67 acres at the corner of Scott Boulevard and North Decatur Road, will include 250 multi-family residential units, 2,000 square feet of retail space, and 15,000 square feet of leasing and amenity space. Decatur Crossing Phase 2, on 15.45 acres immediately to the east of Decatur Crossing Phase 1 will include 450 multi-family residential units, 80,000 square feet of retail space, and 15,000 square feet of office space. In total, the Decatur Crossing projects, located on 20.12 acres, will include 600 multi-family residential units, 90,000 square feet of retail space, 15,000 square feet of office space, and 15,000 square feet of leasing/amenity space.

INFRASTRUCTURE

Transportation

How many site access points and parking facilities will be associated with the proposed development? What are their locations?

The proposed development will have five access points. Three access points are proposed on Scott Boulevard and two access points are proposed on North Decatur Road.

The proposed Phase 1 of the development will have 385 parking spaces in a parking deck. The proposed Phase 2 will have 517 parking spaces to serve the retail part of the development, and 720 spaces for the residential part of the development. This results in a total of 1,622 parking spaces. The Phase 2 site plan notes that a separate, one-story parking deck may also be constructed if warranted by office leasing demands. No additional detail is provided about this structured parking on the site plan.

How much average daily traffic will be generated by the proposed project?

The traffic consultant calculated traffic volumes for the proposed land uses and densities using equations contained in the *Institute of Transportation Engineers' (ITE) Trip Generation Manual, Ninth Edition, 2012*. A total of 9,479 net new daily trips are projected. This includes mixed-use, alternative mode, and pass-by trip reductions. The total gross and net trips generated were listed in Table 1 of the trip generation statement.

Table 1: Trip Generation

Project 1: Fuqua Development									
#	Land Use Type	Size	Ind. Var	ITE Code	Daily	AM In	AM Out	PM In	PM Out
1	Apartment	450	DU	220	2,851	45	179	172	93
2	Grocery	30,000	SF	850	3,067	63	39	163	157
3	General Office	15,000	SF	710	310	37	5	16	79
4	Bank	6,400	SF	912	948	44	33	78	78
5	Specialty Retail	43,600	SF	826	1,903	143	155	55	71
Gross Trips					9,079	332	411	484	478
Less Pass-by Trips					-363	-29	-28	-56	-56
Less Internal Capture					-651	0	-45	0	-77
Less Mode Split		5% of apt trips			-143	-2	-9	-9	-5
Project 1 Net New Trips					7,922	301	329	419	340
Project 2: Northwood Ravin									
1	Apartment	250	DU	220	1,639	25	101	101	54
Less Mode Split		5% of apt trips			-82	-1	-5	-5	-3
Project 2 Net New Trips					1,557	24	96	96	51
TOTAL					9,479	325	425	515	391

Source: Decatur Crossing, DeKalb County, GA, Trip Generation Statement

Summarize the transportation improvements as recommended by the traffic study consultant

A traffic study was not conducted, so no transportation improvements were recommended for the roadway network. However, the site plan shows new traffic signals at the following intersections:

- Scott Boulevard/US 29/US 78/SR 8 at Blackmon Drive
- North Decatur Road at Blackmon Drive

List the transportation improvements that would affect or be affected by the proposed project.

The trip generation statement did not identify any transportation improvements.

Based on a review of the ARC RTP/TIP project list, the following projects were identified:

- DK-344C, Upgrades to approximately 31 signals in DeKalb County
 - This includes the intersection of N Decatur Road at Church Street

- DK-424, US 29 (Scott Boulevard / Lawrenceville Highway) and SR 236 (Hugh Howell Road) signal upgrades at 9 locations
 - This includes the intersection of Scott Boulevard at N Decatur Road

Is the site served by transit? If so, describe type and level of service and how it will enhance or be enhanced by the presence of transit? Are there plans to provide or expand transit service in the vicinity of the proposed project?

MARTA bus route 36 on N Decatur Road serves the site. The MARTA Decatur heavy rail station is approximately 1.5 miles to the south.

What other issues should be considered during the traffic study or in general for the proposed development?

The site plan shows new traffic signals at the following intersections:

- Scott Boulevard/US 29/US 78/SR 8 at Blackmon Drive
- North Decatur Road at Blackmon Drive

The applicant shall ensure that these proposed signals meet all applicable DeKalb County and/or GDOT requirements for signal spacing, signal warrants, and operations.

DECATUR CROSSING PHASES 1 AND 2 DRI
DeKalb County
Natural Resources Division Comments
June 3, 2015

Watershed Protection and Stream Buffers

The proposed project is located on an already developed site in the South River Watershed, which is not a water supply watershed in the Atlanta Region and no Part 5 Environmental Minimum Planning Criteria for water supply watersheds apply.

The USGS coverage for the project area shows no perennial streams on or near the. Any unmapped streams on the property may be subject to the 75-foot DeKalb County stream buffers and the 25-foot State Erosion and Sediment Control Buffer. Any waters of the state on the property will subject to the 25-foot State erosion and sediment control buffer.

Stormwater / Water Quality

The project should adequately address the impacts of the proposed development on stormwater runoff and downstream water quality. During construction, the project should conform to all relevant state and federal erosion and sedimentation control requirements. After construction, water quality will be impacted due to polluted stormwater runoff. ARC has estimated the amount of pollutants produced after the construction of the proposed development. These estimates are based on some simplifying assumptions for typical pollutant loading factors (lbs/ac/yr). The loading factors are based on regional storm water monitoring data from the Atlanta Region with impervious areas based on estimated averages for land uses in the Atlanta Region. Given the proposed coverage shown on the submitted plans, commercial was selected as the use for the entire property. The project is being built on an already developed property with existing impervious surfaces, which will affect the actual increases in loading amounts. Where actual impervious percentages are higher or lower than the estimate, or where the land use varies, the pollutant loads will differ accordingly. The following table summarizes the results of the analysis:

Estimated Pounds of Pollutants Per Year:

Land Use	Land Area (ac)	Total Phosphorus	Total Nitrogen	BOD	TSS	Zinc	Lead
Commercial	20.12	34.41	350.09	2172.96	19777.96	24.75	4.43
TOTAL	20.12	34.41	350.09	2172.96	19777.96	24.75	4.43

Total % impervious 85%

For all new or upgraded on-site detention that is required for this project, the facilities should adhere to the applicable Metropolitan North Georgia Water Planning District 2009 Watershed Management Plan's Stormwater and Watershed Protection Ordinances, as adopted by the City of Decatur. In addition, the design should include stormwater management controls (structural and/or nonstructural) as found in the Georgia Stormwater Management Manual (www.georgiastormwater.com) and meet the stormwater management quantity and quality criteria outlined in the Manual. Where possible, the project should utilize the stormwater better site design concepts included in the Manual.

We would also suggest the following additional measures to help reduce stormwater reduction and provide for its reuse:

- Consider using green spaces and tree planting beds as stormwater controls. These can be designed to provide maximum aesthetic value while also providing for water quality treatment and run-off reduction, potentially reducing the need for larger stormwater facilities and helping to minimize the negative effects of stormwater runoff on streams and water quality.
- Consider using pervious concrete or other pervious materials in parking areas. With the proper substrate, such materials can provide a large storage capacity, which will further help to reduce stormwater runoff.
- Consider including rainwater capture in the project design to provide for landscape irrigation during dry periods.



5185 Peachtree Parkway, Suite 240
Norcross, Georgia 30092
o | 770.368.1399
f | 770.368.1944
w | www.fg-inc.net

May 27, 2015

FUQUA DEVELOPMENT, LLC
Eleven Piedmont Center
3495 Piedmont Rd, NE | Suite 905
Atlanta GA 30305
O: 404.907.1709
D: 404.410.1293

Project: Decatur Crossing, DeKalb County, GA

Subject: ITE Trip Generation Statement

To whomsoever it may concern,

Foresite Group, Inc. was retained by Fuqua Development, Inc. to perform a traffic generation analysis for the proposed mixed-use development in DeKalb County, Georgia. The developments are described in the section below and the proposed site plans are attached to this memo.

Project Description:

Fuqua Development is proposing a development that extends between Scott Blvd and N Decatur Road, to the east of the intersection Scott Blvd, N Decatur Road, and Medlock Road in DeKalb County, GA. The site will consist of the following land uses:

- Grocery Store (30,000 SF)
- Drive-in Bank (6,400 SF)
- Specialty Retail (43,600 SF)
- General Office Space (15,000 SF)
- Apartment Buildings (450 dwelling units)

Additionally, Northwood Ravin is constructing an apartment building that consists of approximately 250 dwelling units in the parcel between Fuqua's proposed development and the intersection of Scott Blvd. / Medlock Road/ N Decatur Road to the west.

Trip Generation and Distribution:

Project trips were calculated using equations contained in the Institute of Transportation Engineers' (ITE) latest *Trip Generation Manual*, Ninth Edition, 2012.

Internal capture and pass-by trip reductions were applied to the trip generation estimates using the methodology prescribed in the ITE (Institute of Transportation Engineers) Trip Generation Handbook, Second Edition, 2004. Internal capture reductions account for vehicles using multiple uses on the mixed-use site without using the external roadway network. The reduction in trips due to internal capture can be seen in Table 1.

Pass-by reductions account for vehicles already on the existing road network that will make an intermediate stop on the way from an origin to a primary trip destination. Pass-by reductions are calculated after internal capture reductions are taken because pass-by only applies to external site trips. Please see Table 1 for the number of pass-by trips.

Mode Split accounts for the trips that were reduced to account for users of alternative modes of transportation such as transit, bicycles, and pedestrians. A 5% reduction in trips was applied to the expected trips from the apartments.

All the trip generation calculations are attached to the memo.

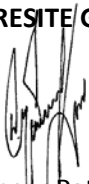
Table 1: Trip Generation

Project 1: Fuqua Development									
#	Land Use Type	Size	Ind. Var	ITE Code	Daily	AM In	AM Out	PM In	PM Out
1	Apartment	450	DU	220	2,851	45	179	172	93
2	Grocery	30,000	SF	850	3,067	63	39	163	157
3	General Office	15,000	SF	710	310	37	5	16	79
4	Bank	6,400	SF	912	948	44	33	78	78
5	Specialty Retail	43,600	SF	826	1,903	143	155	55	71
Gross Trips					9,079	332	411	484	478
Less Pass-by Trips					-363	-29	-28	-56	-56
Less Internal Capture					-651	0	-45	0	-77
Less Mode Split		5%	of apt trips		-143	-2	-9	-9	-5
Project 1 Net New Trips					7,922	301	329	419	340
Project 2: Northwood Ravin									
1	Apartment	250	DU	220	1,639	25	101	101	54
Less Mode Split		5%	of apt trips		-82	-1	-5	-5	-3
Project 2 Net New Trips					1,557	24	96	96	51
TOTAL					9,479	325	425	515	391

Please feel free to contact Blake Bredbenner or myself at (770)-368-1399 or spatharkar@fg-inc.net

Sincerely,

FORESITE GROUP, INC.



Sameer Patharkar, PE
Project Manager

Trip Generation - Decatur Crossing - GA



Calculation of Anticipated Project Trips

Based upon methodology from ITE's *Trip Generation Manual*, 9th Edition (2012)

Copyright 2014, Foresite Group, Inc.

Project Land Use	Project Density	Project Trips			ITE Code	Variable	Equation Used ¹	In/Out Distribution
		Total	Inbound	Outbound				
Specialty Retail Center	43,600 SF				826	Square Feet		
	Daily	1,903	951	952			$T = 42.78(X) + 37.66$	50% / 50%
	AM Peak Hour	298	143	155			$T = 6.84(X)$	48% / 52%
	PM Peak Hour	126	55	71			$T = 2.40(X) + 21.48$	44% / 56%
Reductions for Pass-By Trips								
	Daily	628	314	314				
	AM Peak Hour	45	23	22				
	PM Peak Hour	50	25	25				
TOTAL PROJECT TRIPS								
	Daily	1,275	637	638				
	AM Peak Hour	253	120	133				
	PM Peak Hour	76	30	46				
Drive-In Bank	6,400 SF				912	1,000 S.F.		
	Daily	948	474	474			$T = 148.15(X)$	50% / 50%
	AM Peak Hour	77	44	33			$T = 12.08(X)$	57% / 43%
	PM Peak Hour	156	78	78			$T = 24.30(X)$	50% / 50%
Reductions for Pass-By Trips								
	Daily	313	157	156				
	AM Peak Hour	12	6	6				
	PM Peak Hour	62	31	31				
TOTAL PROJECT TRIPS								
	Daily	635	317	318				
	AM Peak Hour	65	38	27				
	PM Peak Hour	94	47	47				
Supermarket	30,000 SF				850	1,000 S.F.		
	Daily	3,067	1,534	1,533			$T = 102.24(X)^*$	50% / 50%
	AM Peak Hour	102	63	39			$T = 3.4(X)$	62% / 38%
	PM Peak Hour	320	163	157			$\ln(T) = 0.74(X) + 3.25$	51% / 49%
Reductions for Pass-By Trips								
	Daily	767	384	383				
	AM Peak Hour	15	8	7				
	PM Peak Hour	128	64	64				
TOTAL PROJECT TRIPS								
	Daily	2,300	1,150	1,150				
	AM Peak Hour	87	55	32				
	PM Peak Hour	192	99	93				
General Office	15,000 SF				710	1,000 S.F.		
	Daily	310	155	155			$\ln(T) = 0.76 \ln(X) + 3.68$	50% / 50%
	AM Peak Hour	42	37	5			$\ln(T) = 0.80 \ln(X) + 1.57$	88% / 12%
	PM Peak Hour	95	16	79			$T = 1.12 (X) + 78.45$	17% / 83%
Reductions for Pass-By Trips								
	Daily	0	0	0				
	AM Peak Hour	0	0	0				
	PM Peak Hour	0	0	0				
TOTAL PROJECT TRIPS								
	Daily	310	155	155				
	AM Peak Hour	42	37	5				
	PM Peak Hour	95	16	79				
Apartment	450 DU				220	DU		
	Daily	2,851	1,426	1,425			$T = 6.06(X) + 123.56$	50% / 50%
	AM Peak Hour	224	45	179			$T = 0.49(X) + 3.73$	20% / 80%
	PM Peak Hour	265	172	93			$T = 0.55(X) + 17.65$	65% / 35%
Reductions for Pass-By Trips								
	Daily	0	0	0				
	AM Peak Hour	0	0	0				
	PM Peak Hour	0	0	0				
TOTAL PROJECT TRIPS								
	Daily	2,851	1,426	1,425				
	AM Peak Hour	224	45	179				
	PM Peak Hour	265	172	93				
TOTAL GROUP TRIPS					826	Specialty Retail Center		
	Daily	9,079	4,540	4,539	912	Drive-In Bank		
	AM Peak Hour	743	332	411	850	Supermarket		
	PM Peak Hour	962	484	478	710	General Office		
TOTAL INTERNAL CAPTURE TRIPS**					220	Apartment		
	Daily	794	0	0				
	AM Peak Hour	56	2	54				
	PM Peak Hour	91	9	82				
TOTAL PASS-BY TRIPS								
	Daily	1,708	855	853				
	AM Peak Hour	72	37	35				
	PM Peak Hour	240	120	120				
NET NEW EXTERNAL VEHICULAR TRIPS								
	Daily	7,371	3,685	3,686				
	AM Peak Hour	671	295	376				
	PM Peak Hour	722	364	358				

Note: ¹ Where: T = Trips; X = Density by Variable

**Internal capture calculations done separately

Apartment - Northwood Ravin
Calculation of Anticipated Project Trips
Based upon methodology from ITE's Trip Generation, 9th Edition (2012)



Project Land Use	Project Density	Project Trips			ITE Code	Variable	Equation Used ¹	In/Out Distribution
		Total	Inbound	Outbound				
Apartment	250 DU				220	DU		
Daily		1,639	820	819			$T = 6.06(X) + 123.56$	50% / 50%
AM Peak Hour		126	25	101			$T = 0.49(X) + 3.73$	20% / 80%
PM Peak Hour		155	101	54			$T = 0.55(X) + 17.65$	65% / 35%
Reductions for Pass-By Trips								
Daily		0	0	0				
AM Peak Hour		0	0	0				
PM Peak Hour		0	0	0				
TOTAL PROJECT TRIPS								
Daily		1,639	820	819				
AM Peak Hour		126	25	101				
PM Peak Hour		155	101	54				

Note:
¹ Where: T = Trips; X = Density by Variable



VICINITY MAP
SCALE: NTS



ARCHITECTS

THIS DRAWING IS THE PROPERTY OF PIERPER O'BRIEN HERR ARCHITECTS. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. IT IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM.

CURRENT ZONING R-75 & O-1

NOTES:
1. ALL DIMENSIONS ARE IN FEET AND INCHES.
2. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.
3. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
4. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

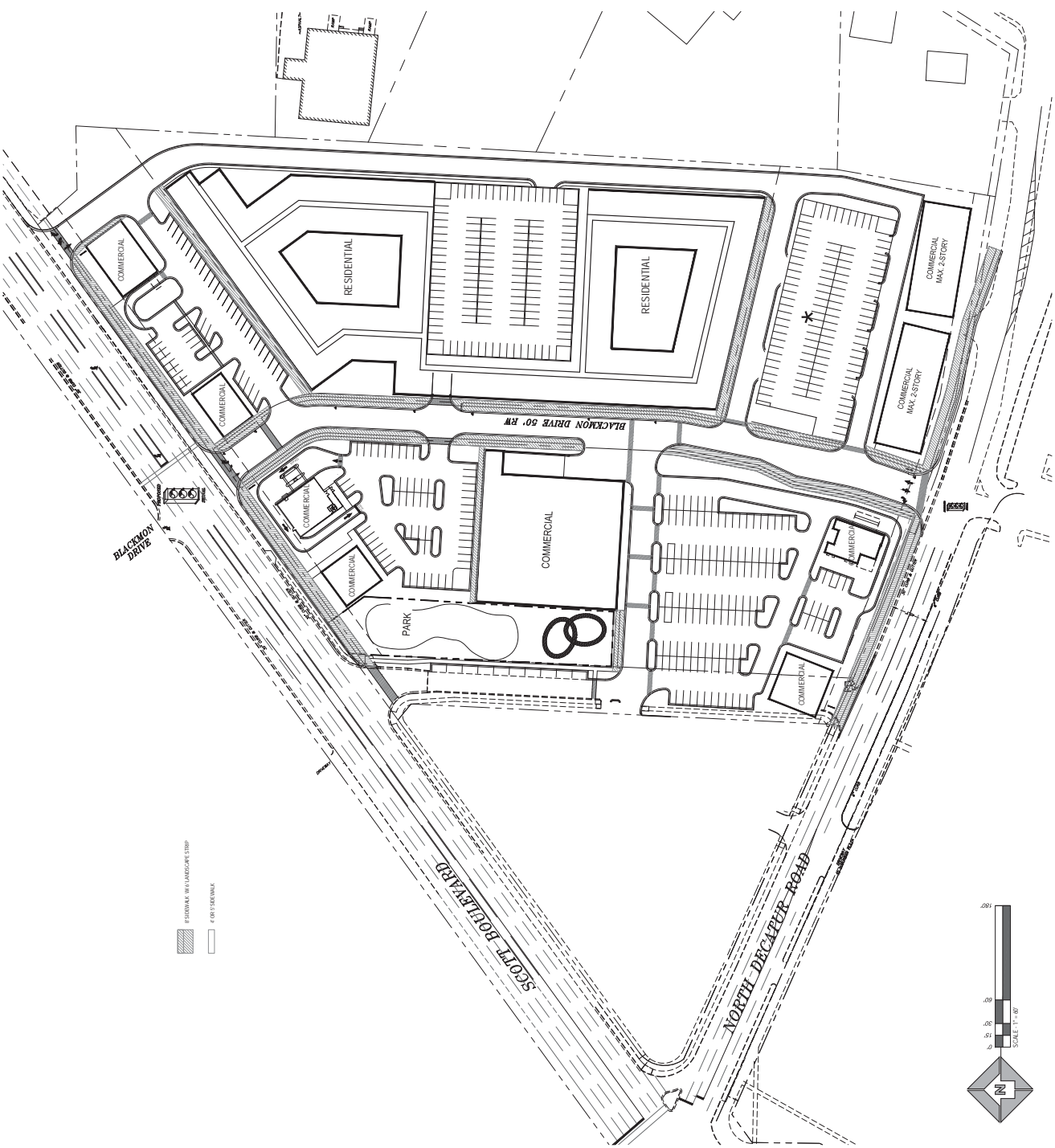
REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.

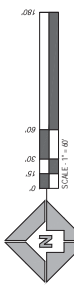
REQUIREMENTS:
1. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.
2. ALL DIMENSIONS ARE TO BE MAINTAINED AT ALL TIMES.



BLACKMON DRIVE 60' RW

SCOTT BOULEVARD

NORTH DECATUR ROAD



NOT RELEASED FOR CONSTRUCTION

NO.	DATE	DESCRIPTION	BY
1	2013.04.10	2013.04.10	2013.04.10
2	2013.04.10	2013.04.10	2013.04.10
3	2013.04.10	2013.04.10	2013.04.10
4	2013.04.10	2013.04.10	2013.04.10
5	2013.04.10	2013.04.10	2013.04.10
6	2013.04.10	2013.04.10	2013.04.10
7	2013.04.10	2013.04.10	2013.04.10
8	2013.04.10	2013.04.10	2013.04.10
9	2013.04.10	2013.04.10	2013.04.10
10	2013.04.10	2013.04.10	2013.04.10

DECATUR CROSSING
DEKALB CO.,
GEORGIA



2013.04.10
26 MAY 2015

ZONING
SITE PLAN

SP-49

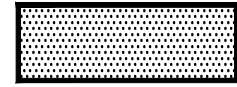
Plotted By: Platinum, Mark Sheet Set: KHA, Layout: C2-00, March 28, 2015, 11:17:37am, K:\A\LP, PRJ\019418004_Scott Blvd, Multi-Family CAD\DWG\Sheet\C2-00 - Site Plan.dwg

PROPOSED BUILDING INFORMATION

BUILDING USAGE:
A 5 STORY, TYPE III MULTI-FAMILY RESIDENTIAL APARTMENT BUILDING w/ AMENITIES AND A 5.5 LEVEL, 384 SPACE PRECAST CONCRETE PARKING DECK. THE SOUTHEAST CORNER WILL HAVE A SMALL PORTION OF THE BUILDING FOR RETAIL USE.

BUILDING DATA:
TOTAL BUILDING AREA -307,577 SF
TOTAL UNITS RESIDENTIAL -250 UNITS
LEASING/AMENITIES -15,500 SF
RETAIL -2,000 SF
BUILDING HEIGHT -5 STORIES

PAVEMENT LEGEND:



ASPHALT PAVEMENT



HEAVY DUTY CONCRETE PAVEMENT



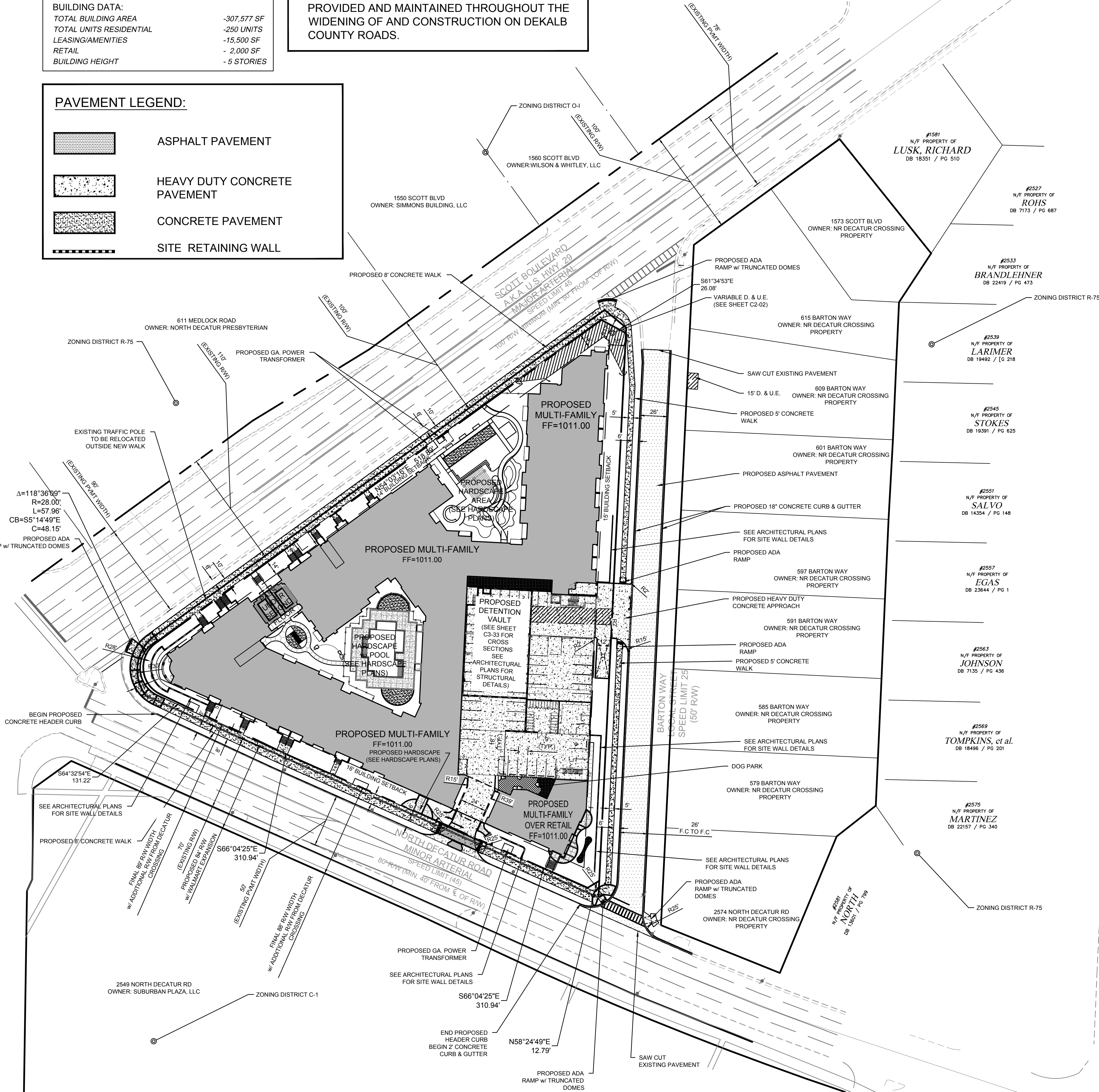
CONCRETE PAVEMENT



SITE RETAINING WALL

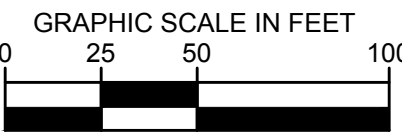
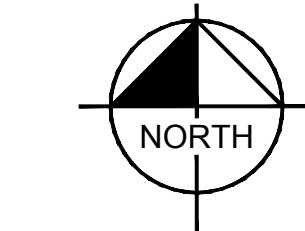
TRANSPORTATION NOTE:

NECESSARY BARRICADES, SUFFICIENT LIGHTS, SIGNS AND OTHER TRAFFIC CONTROL METHODS AS MAY BE NECESSARY FOR THE PROTECTION AND SAFETY OF THE PUBLIC SHALL BE PROVIDED AND MAINTAINED THROUGHOUT THE WIDENING OF AND CONSTRUCTION ON DEKALB COUNTY ROADS.



NOTE:

ALL RETAINING WALLS REQUIRE SEPARATE PERMIT. VENDOR SHALL PROVIDE ALL DESIGN/ MATERIALS/ SPECIFICATIONS FOR MODULAR DESIGN STRUCTURAL ENGINEER SHALL PROVIDE DESIGN FOR REINFORCED CONCRETE WALLS.



B-10000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000000																
---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--

comm.

Item No.

CONDITIONS

Z-13-18724

SLUP-13-18726

~~LP-13-18722~~4/8/14
OK

1. The development shall substantially comply with the site plan dated March 17, 2014, prepared by Pieper, O'Brien, Herr Architects attached as Exhibit "A" (the "Site Plan"), but up to 60 multi-family townhome units total shall be allowed.
2. The development shall contain up to a maximum of 310 dwelling units.
3. The proposed multi-family townhome units may be owned as the following types:
 - a. Condominium units in multi-family, townhome-style buildings, in which the condominium owner has fee-simple interest in the interior of the unit, and all other building areas and components outside the unit and the land outside the exterior of the building are owned and managed in common;
 - b. Fee-simple townhomes, in which the townhome owner has fee-simple interest in the land lying immediately beneath the unit and coincident with the external walls of such unit, and where all land on the exterior of the townhome buildings is owned and managed in common; or
 - c. Rental units in multi-family, townhome-style buildings.
4. The proposed development shall contain a minimum of 2,000 square feet of commercial or retail space, which may include, but is not limited to, a coffee shop or similar use.
5. The height of the proposed multi-family residential building shown on the Site Plan shall be no more than five (5) stories and a basement (which shall not be visible from public right-of-way) and shall not exceed a maximum height of 75' as measured by the Dekalb County Code.
6. The height of the proposed multi-family townhome units shown on the Site Plan shall be no more than three (3) stories, including the garage, and shall not exceed a maximum height of 35' as measured by the Dekalb County Code.
7. The design of the proposed multi-family residential building shown on the Site Plan shall be consistent with the drawing of the Scott Boulevard and North Decatur Road conceptual building elevation prepared by Dwell Design Studio and stamped as received by the Department of Planning and Sustainability on February 21, 2014 and attached as Exhibit "B". Further, landscaping and streetscape for the multi-family residential building shall be consistent with the drawings attached as Exhibits "C" and "D".
8. One ground sign ^{for the multi-family residential buildings} shall be allowed on each street frontage; alternatively, one ground sign shall be allowed at the corner of Scott Boulevard and North Decatur Road. Any such sign shall be designed as a free-standing monument with a maximum height of six feet, measured from sidewalk grade,

Per Sign

and a maximum length of 15 feet. If a sign is placed on each street frontage, the maximum sign face area shall be 32 square feet; if placed at the corner of Scott Boulevard and North Decatur Road, the maximum sign face area shall be 50 square feet. Wall signs shall comply with Chapter 21 of the DeKalb County Code ("the Sign Ordinance"). The graphic design of signage for the multi-family residential building shall be consistent with the drawing attached as Exhibit "D".

for the multi-family residential buildings

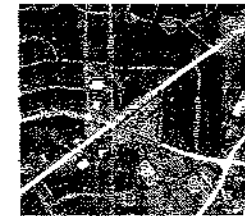
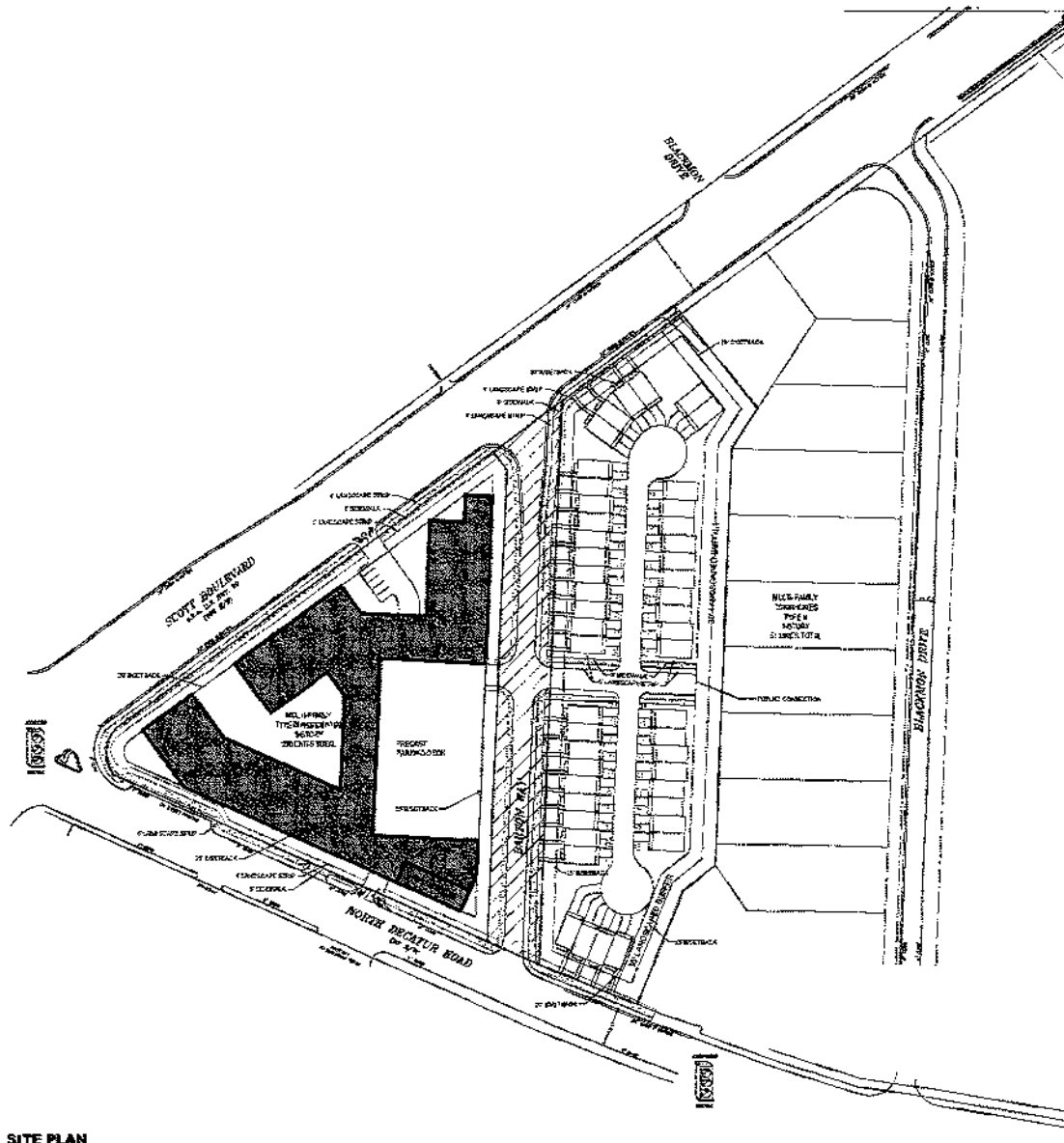
9. The curb cut access on North Decatur Road shown on the Site Plan shall be limited to a right-in, right-out turning movement. Contingent on any necessary approvals, said access shall be designed to prevent left-in, left-out turning movements with the placement of curbs, striping, raised traffic islands or other improvements, or as approved by the Dekalb Transportation Department.
10. The curb cut access on Scott Boulevard shown on the Site Plan shall be limited to a right-in, right-out turning movement. Contingent on any necessary approvals, said access shall be designed to prevent left-in, left-out turning movements with the placement of curbs, striping, raised traffic islands or other improvements, or as approved by the Dekalb Transportation Department.
11. Contingent on any necessary approvals, Developer shall:
 - a. Create a stop area for the Cliff bus along Scott Boulevard; and
 - b. Create a stop area for the MARTA bus along North Decatur Road and install a sheltered bus stop along North Decatur Road.
12. The development shall contain bike racks. Further, benches shall be located in the hardscape area shown on the Site Plan at the intersection of Scott Boulevard and North Decatur Road.
13. The exterior finishes of the multi-family residential building and the multi-family townhome units shall consist of glass, brick, stucco, stacked stone, cementitious siding, or aluminum, and/or any combination thereof.
14. The precast parking deck on the multi-family residential building shall contain a minimum of 25 bike lockers for tenants. The portion of the precast parking deck shown on the Site Plan fronting Barton Way shall include architectural features intended to mimic those on the multi-family residential building shown on the Site Plan provided, however, that should Barton Way be abandoned, this condition relating to architectural features shall no longer apply. The precast parking deck shall be no higher, measured in feet, than the multi-family residential building.
15. Flashing, animated, sound emitting or rotating signs are prohibited.
16. Exterior light poles shall be directed inward and downward and have a controlled footprint intended to minimize light spillover.

- D3
17. Any refuse areas for the proposed multi-family residential building shown on the Site Plan shall be located within the footprint of that building and/or the attached precast parking deck. The multi-family residential building shall provide space and facilities for tenants to recycle paper, glass, metals and plastics. Any refuse areas for the multi-family townhome units shown on the Site Plan shall be screened from view from the public right-of-way and parking areas. Further, no refuse areas for the multi-family townhome units shall be located behind the eastern most multi-family townhome units. The site will contain a pet refuse station on Scott Boulevard and North Decatur Road.
 18. Developer shall not build balconies or decks on the third floor of the eastern-most multi-family townhome units between the eastern side of said multi-family townhome units and the eastern property line.
 19. Developer shall attempt to maintain mature trees in the area described on the Site Plan as the "30' Landscaped Buffer" and, contingent on any necessary approvals, will add plantings to said area to enhance screening and privacy. Developer shall erect a six (6) foot wooden fence along the eastern property boundary prior to issuance of a Certificate of Occupancy for the multi-family townhome units and shall plant Leyland Cypress or an equivalent on the west side of said fence at 15 feet on center, except where impacted by existing mature tree growth.
 20. When demolition of the existing single-family detached homes on Barton Way commences, the single-family detached homes on Barton Way shall be demolished simultaneously, rather than in a piecemeal manner.
 21. Construction activity shall be limited to (1) Monday-Friday, 7:00 a.m. to 7:00 p.m.; and (2) Saturday, 8:00 a.m. to 5:00 p.m. There shall be no construction activity on Sundays, New Year's Day, Thanksgiving Day, Christmas Day, Memorial Day, July 4th or Labor Day unless such activity arises from an emergency which puts the site or neighboring property owners and their property at risk of harm or loss.
 22. Developer shall follow the "Best Practice Management for Erosion Guidelines."
 23. The site will be provided with underground utilities for electric, power, phone, cable and internet.
 24. Prior to a LDP being issued for the multi-family residential building, the Developer shall submit to the County Arborist a landscape plan that substantially conforms to the standards of Exhibit "C" hereto. The Arborist shall approve the proposed tree selection and placement. Such landscape plan will define the landscaping plan for the multi-family residential building and the west-side of Barton Way. Further, prior to a LDP being issued for the multi-family townhome units, the Developer shall submit to the County Arborist a landscape plan that defines the landscaping for the multi-family townhome units and the east-side of Barton Way. The Arborist shall approve the proposed tree selection and placement.

D3

25. Developer shall abide by the Dekalb County Tree Ordinance.
26. Developer shall employ pest and rodent abatement measures during demolition.
27. Developer shall employ dust abatement measures during demolition.
28. Water Quality shall meet the minimum requirements of Georgia Stormwater Management Manual or Dekalb County.
29. Setbacks shall be measured from existing right-of-way, versus any future right-of-way the County may demand incidental to development.
30. Parts of the property may be owned separately and/or conveyed as separate tracts and to separate owners with different ownership structures.
31. Should there be separate ownership of parts of the property and/or a conveyance of part of the property to different owners, all conditions and variances shall remain applicable to any portion of the property, regardless of what person or entity owns the property.
32. Separate ownership of parts of the property and/or conveyance of part of the property to different owners shall not create non-conforming uses and shall not require any changes to the Site Plan for the development nor any variances. The newly-created property lines which arise from these conveyances are not required to observe setback, buffer or other zoning requirements, except as may be reflected on Site Plan.

April 8, 2014




**PIEPER
O'BRIEN
HERR**
ARCHITECTS

SITE CALCULATIONS			
Site Area		270,543 S.F.	6.220 Acres
BUILDING COVERAGE			
Proposed Buildings		304,798 S.F.	
Paved Areas		28,388 S.F.	
TOTAL PROPOSED COVERAGE		333,176 S.F.	less than 50%
PUBLIC SPACE CALCULATION			
Building Footprints Multi-family		30,750 S.F.	
Building Footprints Townhomes		45,000 S.F.	
Total Paved Area		47,338 S.F.	
Total Building Area		159,041 S.F.	
Net Open Space		118,242 S.F.	greater than 25%
SETBACKS			
Setbacks		As Shown	
LAND USE			
Multi-family		237,260 S.F.	5 stories
Townhomes		45,000 S.F.	3 stories (85')
Retail		2,800 S.F.	High-Walks 22' +
Lodging/Assembly		15,500 S.F.	n/a
Total		220,000 S.F.	
PARKING			
3.5 sp/unit		412 SP	258 units + 88 t/c
Retail @ 5/2000		10 SP	2000 S.F.
Total Parking Shown		502 SP	

NOT RELEASED FOR CONSUMER PROTECTION

REV. NO.	DATE	DESCRIPTION	REV.
1	1/1/80	INITIAL DESIGN	1
2	2/1/80	REVISED DESIGN	1
3	3/1/80	REVISED DESIGN	1
4	4/1/80	REVISED DESIGN	1
5	5/1/80	REVISED DESIGN	1

PROPOSED
DEVELOPMENT
DEKALB CO.,
GEORGIA



McGraw-Hill

20-13041.00
17 MAR 2014

SITE PLAN

SP-7

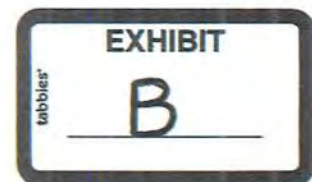




07 FEBRUARY 2014

SCOTT BLVD
ATLANTA, GEORGIA

dwell design
studio



SITE DATA

RESIDENTIAL DATA

5 STORY, TYPE III RESIDENTIAL APARTMENTS
250 UNITS
2,000 SF RETAIL
15,000 LEASING/AMENITY

PARKING DATA

5.5 LEVEL 385 SPACES
PRECAST CONCRETE PARKING DECK

250 UNITS X 1.5 SPACE / UNIT = 375
2,000 SF RETAIL @ 5 / 1000 = 10
SPACES REQUIRED = 385



DENSITY STUDY

SCALE: 1" = 40'-0"



07 FEBRUARY 2014

SCOTT BLVD
ATLANTA, GEORGIA

dwell design
studio

EXHIBIT

C

1 of 2



PARK & POOL COURTYARD ENTRY



CORNER PARK AREA



CORNER PLAZA

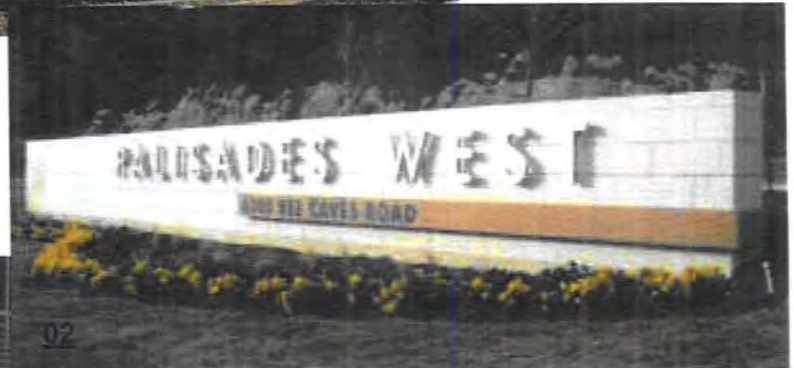


RETAIL PLAZA

07 FEBRUARY 2014

SCOTT BLVD
ATLANTA, GEORGIA

dwell design studio



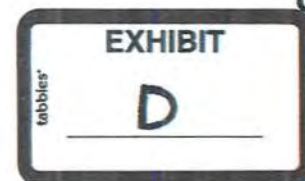
**BUILDING WALL MOUNTED
BLADE SIGN EXAMPLES**

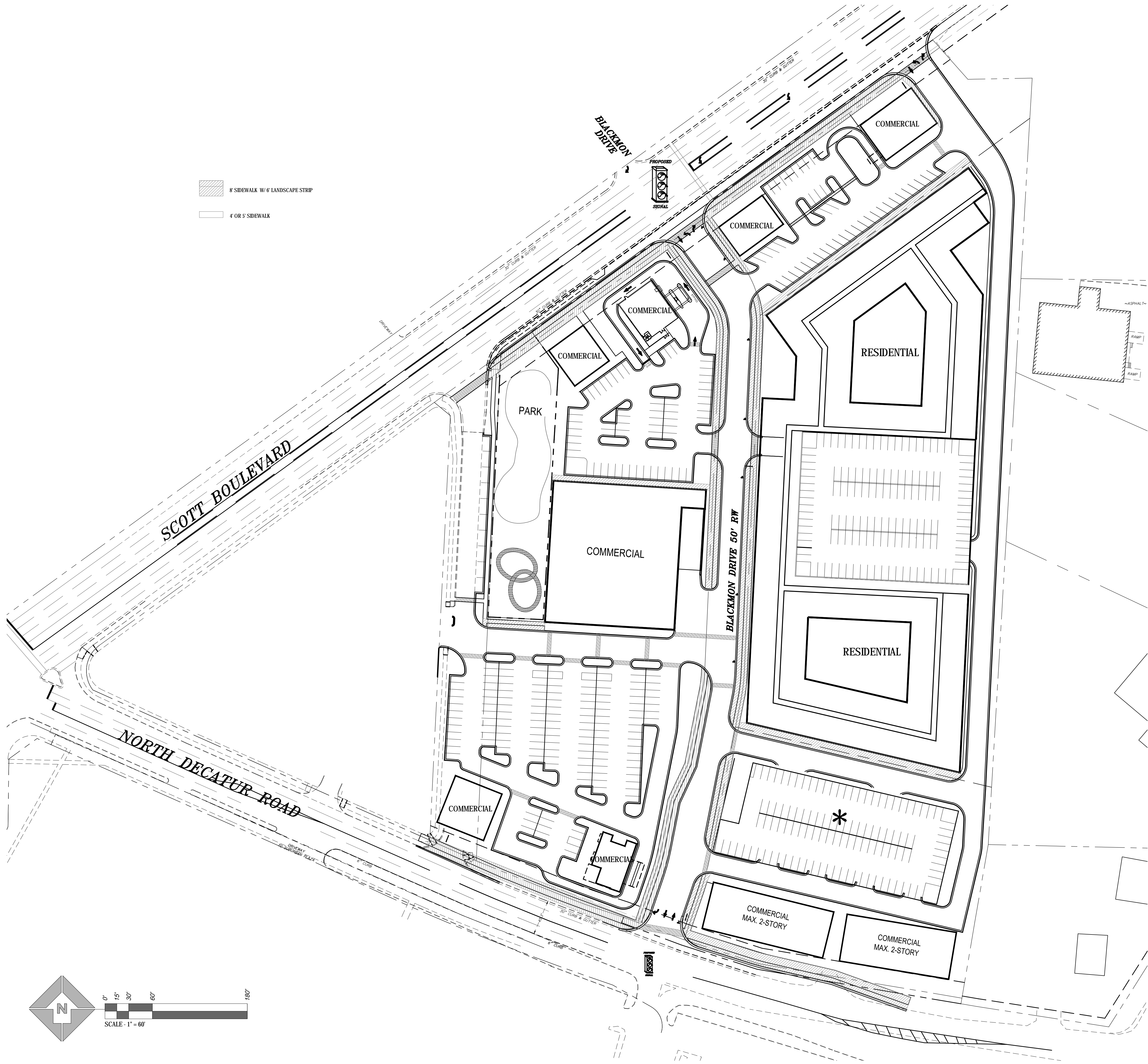
07 FEBRUARY 2014

SCOTT BLVD
ATLANTA, GEORGIA

**GROUND MOUNTED
LANDSCAPE WALL SIGN EXAMPLES**

dwell design
studio



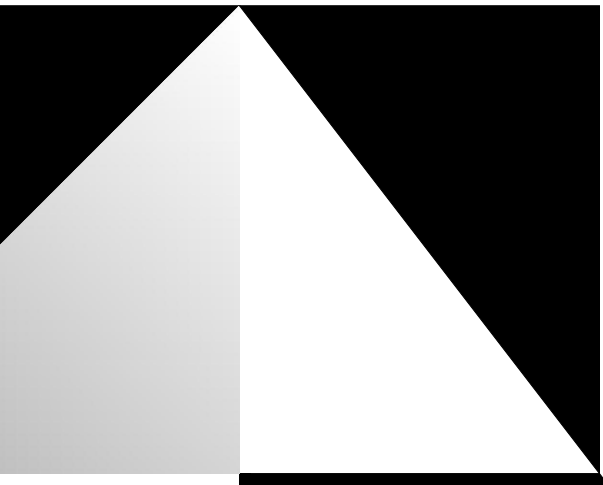


VICINITY MAP
SCALE: NTS

CURRENT ZONING R-75 & O-I

SITE CALCULATIONS			
Site Area	673,002 S.F.	15.45 Acres	
BUILDING COVERAGE			
Proposed Buildings	250,690 S.F.		
Parking Areas	268,220 S.F.		
TOTAL PROPOSED COVERAGE	518,910 S.F.	less than 78%	
PUBLIC SPACE CALCULATIONS			
Building Footprints	250,690 S.F.		
Total Paved Area	268,220 S.F.		
Total Building Area	518,910 S.F.		
Net Open Space	154,092 S.F.	greater than 22%	
SETBACKS			
Req'd Setbacks per OCR	Req'd		
Front	50'	20' Proposed	
Side	20'	20' Proposed	
Rear	40'	50' Proposed	
BUILDING USE			
Retail	80,000 S.F.		
Office	15,000 S.F.		
Total	95,000 S.F.		
Residential	450 UNITS	29 Units/Acre	
PARKING			
OCR Parking Requirements	Required	Provided	
Shopping Center @ 4/1000	380 SP	517 sp = 5.44/1000	
Residential @ 1.75 per unit	788 SP	720 sp = 1.50/unit	
		45 guest parking	
		4 employee	

THE ONE LEVEL OF STRUCTURED PARKING MARKED WITH AN * WILL ONLY BE CONSIDERED IF WARRANTED BY OFFICE LEASING DEMANDS.



PIEPER
O'BRIEN
HERR
ARCHITECTS

THIS DRAWING IS THE PROPERTY OF PIEPER O'BRIEN HERR ARCHITECTS LTD AND IS NOT TO BE REPRODUCED IN WHOLE OR IN PART. IT IS NOT TO BE USED ON ANY OTHER PROJECT AND SHALL BE RETURNED UPON REQUEST. COPYRIGHT AS DATED PIEPER O'BRIEN HERR ARCHITECTS LTD.
NOT VALID UNLESS SIGNED AND SEALED.

NOT RELEASED FOR CONSTRUCTION

APPROVAL

REVISIONS

REV. NO	DATE	DESCRIPTION	BY

DECATUR
CROSSING
DEKALB CO.,
GEORGIA



PROJECT NO. 20-13041.00
DATE 26 MAY 2015
SHEET TITLE

ZONING
SITE PLAN

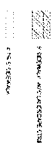
SHEET NO. SP-49

CONDITIONS
(Z-15-19439)

1. The development shall substantially comply with the site plan dated May 26, 2015, prepared by Pieper, O'Brien, Herr Architects and attached as Exhibit "A" (the "Site Plan") and the notes on said Site Plan. However, should the Applicant or its successors, successors - in - title, affiliates or assigns not acquire fee simple title to the subject property, 60 multi-family townhome units as approved in Z-13-18724 shall be allowed pursuant to a Minor Modification to the Site Plan.
2. The development shall contain a maximum of 450 dwelling units.
3. The multi-family residential building shall be no more than five (5) stories and a basement and shall not exceed a maximum height of 75' as measured by the DeKalb County Code. Further, the attached parking deck shall be no higher, measured in feet, than the residential portions of said building.
4. Any refuse areas for the proposed multi-family residential building shown on the Site Plan shall be located within the footprint of that building and/or the attached parking deck. The multi-family residential building shall provide space and facilities for tenants to recycle paper, glass, metals and plastics. Further, the parking deck for the multi-family residential buildings shall contain a minimum of 35 bike lockers for tenants. Finally, the multi-family residential shall provide a dog walk.
5. The proposed development shall have approximately of 95,000 square feet of commercial space, which may include, but not be limited to, retail stores, banks, restaurants, and offices. Drive-thrus on the Subject Property shall not have direct, curb-cut access onto Scott Boulevard or North Decatur Road. Further, the drive-thru restaurant shall be limited to an upscale quick service concept restaurant, including but not limited to a bakery/café.
6. The stories of the non-residential buildings shall be delineated by windows, belt courses, cornices or similar architectural detailing.
7. A minimum of 40% of each non-residential building façade directly fronting Scott Boulevard or North Decatur Road shall consist of fenestration and door openings. All such fenestration shall:
 - a. Begin at a point not more than three (3) feet above the sidewalk, to a height no less than ten (10) feet above the sidewalk; or
 - b. Begin at the finished floor elevation to a height no less than ten (10) feet above the finished floor elevation when the finished floor elevation is three (3) feet or more above the sidewalk; or

- c. Begin at a point not more than sidewalk level, to a height no less than ten (10) feet above the finished floor elevation when the finished floor elevation is below the sidewalk.
8. The exterior finishes of the buildings shall consist of glass, brick, stucco, stacked stone, cementitious siding, or aluminum, and/or any combination thereof.
9. Roof mounted mechanical equipment and appurtenances shall be located or screened so that they are not visible from the immediately adjacent ground level. Screening materials shall be compatible with the surrounding building materials and architectural design.
10. The development shall be allowed three (3) ground signs. The development may place (a) one (1) ground sign not to exceed 15 feet in height on North Decatur Road for the commercial and/or office users; (b) one (1) ground sign not to exceed 15 feet in height on Scott Boulevard for the commercial and/or office users; and one (1) ground sign not to exceed six (6) feet in height on either Scott Boulevard or North Decatur Road for the multi-family building. This condition is not intended to restrict or prohibit and does not restrict or prohibit signage internal to the development or wall signage or window signage.
11. The site shall be provided with underground utilities for electricity, phone, cable and internet services.
12. Two (2) pet refuse stations shall be provided in the park shown on the Site Plan. Further, the park shall include benches, an artistic and/or water feature, recycling receptacles and trash receptacles.
13. The development shall provide for safe pedestrian crossings internal to the site through the use of signage, cross-walks, speed-tables and/or similar features, subject to any necessary approvals.
14. Sidewalks shall be continued across curb cuts on North Decatur Road and Scott Boulevard by differentiating the paving materials, color, or texture of the pedestrian crossing from that of the street or private drive, subject to any necessary approvals.
15. Contingent on any necessary approvals, the developer shall:
 - a. Create a stop area for the Cliff bus along Scott Boulevard or interior to the development; and
 - b. Create a stop area for the MARTA bus along North Decatur Road.
16. The development shall contain a bike rack at the front entrance of the grocery store and at points along Scott Boulevard and North Decatur Road.
17. Benches shall be located in the hardscape areas shown on the Site Plan.

18. Developer shall follow the “Best Practice Management for Erosion Guidelines.”
19. Developer shall abide by the Dekalb County Tree Ordinance.
20. Developer shall employ pest and rodent abatement measures during demolition.
21. Developer shall employ dust abatement measures during demolition.
22. Water Quality shall meet the minimum requirements of Georgia Stormwater Management Manual or Dekalb County.
23. Setbacks shall be measured from existing right-of-way, versus any future right-of-way the County may demand incidental to development.
24. Parts of the property may be owned separately and/or conveyed as separate tracts and to separate owners with different ownership structures.
25. Should there be separate ownership of parts of the property and/or a conveyance of part of the property to different owners, all conditions and variances shall remain applicable to any portion of the property, regardless of what person or entity owns the property.
26. Separate ownership of parts of the property and/or conveyance of part of the property to different owners shall not create non-conforming uses and shall not require any changes to the Site Plan for the development nor any variances. The newly-created property lines which arise from these conveyances are not required to observe setback, buffer or other zoning requirements, except as may be reflected on Site Plan.



VICINITY MAP
SCALE: NTS

CURRENT ZONING R-75 & O-1

SITE CALCULATIONS

Building	Area	Year	Use	Notes
Building 1	200,000 sq. ft.	1980	Office	Renovated
Building 2	150,000 sq. ft.	1985	Warehouse	Renovated
Building 3	100,000 sq. ft.	1990	Office	Renovated
Building 4	80,000 sq. ft.	1995	Office	Renovated
Building 5	60,000 sq. ft.	2000	Office	Renovated
Building 6	40,000 sq. ft.	2005	Office	Renovated
Building 7	20,000 sq. ft.	2010	Office	Renovated
Building 8	10,000 sq. ft.	2015	Office	Renovated
Building 9	5,000 sq. ft.	2020	Office	Renovated
Building 10	2,500 sq. ft.	2025	Office	Renovated
Building 11	1,250 sq. ft.	2030	Office	Renovated
Building 12	625 sq. ft.	2035	Office	Renovated
Building 13	312 sq. ft.	2040	Office	Renovated
Building 14	156 sq. ft.	2045	Office	Renovated
Building 15	78 sq. ft.	2050	Office	Renovated
Building 16	39 sq. ft.	2055	Office	Renovated
Building 17	19 sq. ft.	2060	Office	Renovated
Building 18	9 sq. ft.	2065	Office	Renovated
Building 19	4 sq. ft.	2070	Office	Renovated
Building 20	2 sq. ft.	2075	Office	Renovated
Building 21	1 sq. ft.	2080	Office	Renovated
Building 22	0.5 sq. ft.	2085	Office	Renovated
Building 23	0.25 sq. ft.	2090	Office	Renovated
Building 24	0.125 sq. ft.	2095	Office	Renovated
Building 25	0.0625 sq. ft.	2100	Office	Renovated
Building 26	0.03125 sq. ft.	2105	Office	Renovated
Building 27	0.015625 sq. ft.	2110	Office	Renovated
Building 28	0.0078125 sq. ft.	2115	Office	Renovated
Building 29	0.00390625 sq. ft.	2120	Office	Renovated
Building 30	0.001953125 sq. ft.	2125	Office	Renovated
Building 31	0.0009765625 sq. ft.	2130	Office	Renovated
Building 32	0.00048828125 sq. ft.	2135	Office	Renovated
Building 33	0.000244140625 sq. ft.	2140	Office	Renovated
Building 34	0.0001220703125 sq. ft.	2145	Office	Renovated
Building 35	0.00006103515625 sq. ft.	2150	Office	Renovated
Building 36	0.000030517578125 sq. ft.	2155	Office	Renovated
Building 37	0.0000152587890625 sq. ft.	2160	Office	Renovated
Building 38	0.00000762939453125 sq. ft.	2165	Office	Renovated
Building 39	0.000003814697265625 sq. ft.	2170	Office	Renovated
Building 40	0.0000019073486328125 sq. ft.	2175	Office	Renovated
Building 41	0.00000095367431640625 sq. ft.	2180	Office	Renovated
Building 42	0.000000476837158203125 sq. ft.	2185	Office	Renovated
Building 43	0.0000002384185791015625 sq. ft.	2190	Office	Renovated
Building 44	0.00000011920928955078125 sq. ft.	2195	Office	Renovated
Building 45	0.000000059604644775390625 sq. ft.	2200	Office	Renovated
Building 46	0.0000000298023223876953125 sq. ft.	2205	Office	Renovated
Building 47	0.00000001490116119384765625 sq. ft.	2210	Office	Renovated
Building 48	0.000000007450580596923828125 sq. ft.	2215	Office	Renovated
Building 49	0.0000000037252902984619140625 sq. ft.	2220	Office	Renovated
Building 50	0.00000000186264514923095703125 sq. ft.	2225	Office	Renovated
Building 51	0.000000000931322574615478515625 sq. ft.	2230	Office	Renovated
Building 52	0.000000000465661287307739279375 sq. ft.	2235	Office	Renovated
Building 53	0.0000000002328306436538696396875 sq. ft.	2240	Office	Renovated
Building 54	0.00000000011641532182693481984375 sq. ft.	2245	Office	Renovated
Building 55	0.000000000058207660913467409421875 sq. ft.	2250	Office	Renovated
Building 56	0.0000000000291038304567337047109375 sq. ft.	2255	Office	Renovated
Building 57	0.00000000001455191522836685235546875 sq. ft.	2260	Office	Renovated
Building 58	0.000000000007275957614183426177734375 sq. ft.	2265	Office	Renovated
Building 59	0.0000000000036379788070917130888671875 sq. ft.	2270	Office	Renovated
Building 60	0.00000000000181898940354585654443359375 sq. ft.	2275	Office	Renovated</

THE ONE LEVEL OF STRUCTURED PARKING MARKED WITH AN "M" WILL ONLY BE CONSIDERED IF WARRANTED BY OFFICE LEASING DEMANDS.

EXHIBIT

tabbies*

DECATUR
CROSSING
DEKALB CO.,
GEORGIA

PIE PER
O'BRIEN
HERR /

ARCHITECTS

[illegible]

NOT RELEASED FOR CONSTRUCTION

REV. NO.	DATE	DESCRIPTION	BY
01	01/01/00		

Future Development
20-13041.00
26 MAY 2011
ZONING
SITE PLAN

SP-49